



Report of the Chief Planning Officer

PLANS PANEL SOUTH & WEST

Date: 28th March 2013

Subject: APPLICATION 12/04929/RM- Reserved Matters approval for Siting, External Appearance, Scale and Landscaping for 331 dwellings including internal highways, landscaping and 2 retail units, Former Clariant Works, Calverley Lane, Horsforth

APPLICANT	DATE VALID	TARGET DATE
Redrow Homes Yorkshire	20.11.2012	19.02.2013

Electoral Wards Affected:

Horsforth

Yes Ward Members consulted
(referred to in report)

Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

RECOMMENDATION:

- 1. GRANT PLANNING PERMISSION** subject to conditions listed below.

List of planning conditions:

1. Approval of plans
2. Samples of all external walling, roofing materials to be approved prior to commencement of development
3. Tree protection measures for existing trees
4. No change in levels in root protection areas of retained trees
5. Replacement tree planting if landscaping fails within 5 years of planting.
6. Means of access shall only be as shown on the approved plans
7. Development shall not commence until details of the proposed method of closing off and making good all existing redundant accesses to the development site have been submitted to and approved in writing by the local planning authority.
8. Prior to commencement details of refuse, cycle and motorcycle facilities for flats units to be submitted and approved by the LPA.

9. No construction operation shall take place before 07.30 hours on weekdays and 08.00 hours on Saturdays or after 19.00 hours on weekdays and 13.00 hours on Saturdays with no operation on Sundays or Bank Holidays
10. Surface water drainage strategy to be submitted prior to the commencement and implanted in accordance with approved details.
11. A drainage feasibility study shall be carried out by the developer then submitted to and approved in writing by the Local Planning Authority prior to the commencement of development . The development shall then be carried out in accordance with the findings of the study .
12. Household thresholds should be designed to be sufficiently above ground level to prevent shallow flood flows from flowing directly into buildings
13. There shall be no raising of ground levels in the green space in the bottom South West corner of the site, which is within flood plain.
14. The combined noise from fixed plant shall not exceed a rating level as defined by BS4142 by more than 5dB(A) below the lowest background (L90) during which the plant will operate. Details of said plant shall be submitted to the Local Planning Authority and approved in writing prior to occupation.
15. Off site highway works for waiting restrictions on Low Hall Road and Calverley Lane to be submitted and approved prior to first occupation. Implementation to be agreed.
16. Land dedicated at North West corner adjacent to Low Hall Road to facilitate improvements to pedestrian access over bridge/culvert.
17. Garages to be only used for storage of motor vehicles.
18. Parking to be laid out for relevant phase prior to occupation.
19. The vehicular access gradient shall not exceed 1 in 40 (2.5%) for the first 15m and 1 in 20 (5%) thereafter, unless otherwise agreed in writing by the Local Planning Authority. The gradient of the pedestrian access shall not exceed 1 in 20 (5%).
20. The gradient of all drives shall not exceed 1 in 12.5 (8%).
21. The development hereby approved shall not be brought into use until works have been undertaken to provide the sightlines shown on the approved plan. These sight lines shall be retained clear of all obstructions to visibility greater than 1m in height above the adjacent carriageway for the lifetime of the development.

In reaching a decision the case officer dealing with the application has worked with the applicant/agent in a positive way to produce an acceptable scheme in accordance with paragraphs 186 and 187 of the National Planning Policy framework.

In granting permission for this development the City Council has taken into account all material planning considerations including those arising from the comments of any statutory and other consultees, public representations about the application and Government Guidance and Policy as detailed in the National Planning Policy Framework and (as specified below) the content and policies within Supplementary Planning Guidance (SPG) and The Development Plan consisting of the Leeds Unitary Development Plan Review 2006 (UDPR), the Natural Resources and Waste Local Plan 2013 (NRWLP) and the emerging Publication Draft Core Strategy Nov 2012 (DCS).

GP5, BD5, N2, N12, N13, T2, T24, LD1, H4
Neighbourhoods for Living SPG
Street Design Guide SPD

On balance, the City Council considers the development would not give rise to any unacceptable consequences for the environment, community or other public interests of acknowledged importance.

1.0 INTRODUCTION:

- 1.1 This application is presented to Plans Panel due to the size and scale of the proposed development.
- 1.2 Members may recall that in March 2011 the Outline planning applications for the redevelopment of the Clariant and Riverside Mills sites were presented before Panel. Members resolved to refuse the two applications on the grounds that the sites were located in unsustainable locations and would be reliant on private car travel for future occupiers and that the travel planning measures were not sufficient to make the development acceptable in sustainability terms. They were also concerned with the impact on highway safety from the use of Calverley Lane (South) to link to the Outer Ring Road. The applicants lodged an appeal against the refusals and Public Inquiry was held. Members were briefed in July 2012 on the decision by the Secretary of State to allow the appeals and grant Outline planning permission for the redevelopment of both the Clariant and Riverside Mills site for housing.
- 1.3 The application before Members is for the Clariant site. The Outline approval allowed for up to 400 dwellings to be created on site. This Reserved Matters application is for 331 dwellings and 2 retail units. The Outline planning permission for the site was granted with the Access details which include a main vehicular access on to Calverley Lane utilising the existing access into the former Clariant site and a second vehicular access from the site on to Low Hall Road.
- 1.4 The Reserved Matters before Panel seeks permission for the detailed layout of the site, the design and appearance of the houses and the laying out of the landscape areas.

2.0 PROPOSAL:

- 2.1 The proposal is for the erection of 331 dwellings and 2 retail units along with internal road layout and areas of public open space.
- 2.3 The dwellings would be 2 storeys in height some would have accommodation in the roof. The dwellings are predominantly semi-detached and detached. There are a few small terrace rows. There are a range of house sizes from 2, 3, 4 and 5 bedroom properties. The retail units are located in the centre of the site and would have flats above them. These are the only 3 storey buildings proposed on site.
- 2.4 The dwellings would be of traditional design, taking reference from the surround area with a good mixture of pitched and gable roof styles. The palette of materials would be predominantly artificial stone and slate. There would be some natural stone and slate properties located around site boundary with Calverley Lane and Low Hall Road. Render would also be used on some plots throughout the site.
- 2.5 There would 3 main areas of public open space created on the site. Within the site a new internal road system would be constructed with the main spine route being widen enough to accommodate a bus service required as part of the Outline

planning permission which could enter and exit the site via Calverley Lane. Along the main internal spine route the carriageway width is widened enough to allow the planting of street trees on one side of the carriage way to help create a sense of place.

3.0 SITE AND SURROUNDINGS:

- 3.1 The Clariant site comprises a former chemical production/treatment works (c 12.5 ha) located on the west side of Calverley Lane, Horsforth. The site has been cleared of the former Clariant works and is currently being prepared for the development.
- 3.2 The Clariant site is currently accessed from the Ring Road (A6110) at Calverley Lane South and Calverley Lane North (between the Horsforth and Rodley roundabouts).
- 3.3 The site is surrounded by areas of green belt (comprising open fields and mature vegetation), the River Aire, Leeds & Liverpool Canal and a railway line.
- 3.4 The Horsforth Conservation Area is along Low Hall Road over the Culverted bridge outside of the Clariant site

4.0 RELEVANT PLANNING HISTORY:

- 4.1 10/04068/OT: CLARIANT SITE,– Outline Application including means of access to erect residential development for up to 400 dwellings with associated public open space, parking landscaping, ancillary retail unit, allotments, retention of sports ground with pavilion and associated off-site highway works. Refused by Panel March 2011 but allowed on appeal 2012.
- 4.2 10/04261/OT: RIVERSIDE MILLS– Outline Application including means of access to erect residential development for up to 150 dwellings with associated public open space and off-site highway works. Refused by Panel March 2011 but allowed on appeal 2012.

5.0 HISTORY OF NEGOTIATIONS:

- 5.1 The application has been the subject of pre-application discussions before submission of the mixed use application. The applicant also held a community drop in event, placed notices and plans in Horsforth and Calverley Library and briefed both sets of Ward Councillors. Councillors Collins and Cleasby have both expressed their satisfaction with the revised masterplan details.

6.0 PUBLIC/LOCAL RESPONSE:

- 6.1 The application has been publicised by means of site notices. There have been a 19 representations received of which 16 are objecting. The following issues have been raised:

- Flood Risk Assessment and Phase 2 report missing
- The scheme should utilise SUDs
- The highways situation should be re-reviewed
- This is an unsustainable site, no local amenities
- More cars on the ring road
- Local schools are over subscribed as are doctors

Dangerous road junction
The site is a former chemical works that may be toxic
Too many houses
Concern over disruption during construction period
The development will have a negative effect upon local communities
The proposals should include a footbridge over the Ring Road.
Greenspace area is too small
No sense of place is being created

- 6.2 The Leeds Civic Trust object to the proposal on the grounds of the impacts on the highway network. They do not consider the bus service proposed will be sustainable in the long term. The design of the properties is nothing special and the majority of the properties are 3 bedroom. The village green area is smaller than shown on the Outline plans. The design appears to only be doing the minimal when it comes to 'green' provision and renewable energy. We would like to see this as a sustainable village and set an example for future developments.

7.0 CONSULTATION RESPONSES:

- 7.1 Highways – no objections subject to conditions.
- 7.2 Mains Drainage – no objections subject to conditions.
- 7.3 Environment Agency – No objections
- 7.4 Coal Authority – No objections
- 7.5 Sport England – Have an objection to the proposals at present due to the lack of information submitted in relationship to the sports ground and pavilion scheme. The developer has supplied further information about the works to the pavilion and also the proposals for the pavilion and sports pitches maintenance and funding. This information has been provided to Sport England and further discussions will take place. The outcome of those discussions will be brought to Panel with a verbal update.

8.0 PLANNING POLICIES:

- 8.1 As required by Section 38(6) of the Planning and Compulsory Purchase Act 2004 this application has to be determined in accordance with the Development Plan unless material considerations indicate otherwise.
- 8.2 The most relevant Policies in the adopted Leeds Unitary Development Plan are outlined below.

Policy GP5 refers to detailed planning considerations and states that development proposals should seek to avoid loss of amenity.

Policy BD5 refers to new building design

Policy N2 refers to Greenspace in new developments

Policies N12 and N13 refer to the good urban design considerations and placing making

Policies T2 and T24 seek to maintain adequate vehicle access and levels of vehicle parking provision with no undue detriment to other highway users.

Policies H4 Refers to Windfall housing proposals

Neighbourhoods for Living SPG.

8.3 National Planning Policy Guidance:

The National Planning Policy Framework came into effect on 27th March 2012, and replaces the advice provided in Planning Policy Guidance Notes and Statements. The aim of this document is to make the planning system less complex and more accessible, to protect the environment and to promote sustainable growth. Local planning authorities are expected to “plan positively” and that there should be a presumption in favour of sustainable development:

“At the heart of the planning system is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan-making and decision-taking.” (para 14).

8.4 The Government’s pursuit of sustainable development involves seeking a wide variety of positive improvements including:

1. making it easier for jobs to be created in cities, towns and villages
2. replacing poor design with better design
3. improving the conditions in which people live, work, travel and take leisure

Emerging Core Strategy

The Publication Draft of the Core Strategy was issued for public consultation on 28th February 2012 with the consultation period closing on 12th April 2012. Following consideration of any representations received, the Council intends to submit the draft Core Strategy for examination. The Core Strategy sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. As the Core Strategy is in its pre submission stages only limited weight can be afforded to any relevant policies at this point in time.

POLICY P10: DESIGN

New development for buildings and spaces, and alterations to existing, should be based on a thorough contextual analysis to provide good design appropriate to its scale and function.

9.0 MAIN ISSUES:

9.1 The following main issues have been identified:

- Design, scale, siting and appearance of houses and masterplan layout
- Neighbours amenity and future occupiers
- Highway considerations
- Landscaping
- Other considerations

10.0 APPRAISAL:

- 10.1 The site comprises previously developed land. The site benefits from Outline planning permission that could allow up to 400 dwellings to be constructed on site. This number is derived from the highways assessments appraised during the Outline application stage. The developer is proposing to build 331 dwellings on site and 2 small retail units along with laying out of internal roads and greenspace areas. This application relates to the detail of the masterplan and the design of the dwellings. **The principle of the development, the number of units that can be built and the means of access from the site along with the impact on the local highway network and schools has been determined at the Appeal stage and is not for consideration in this application.**

Urban design considerations

- 10.2 The proposed masterplan is designed to provide a new housing settlement that sits within a semi rural location surrounded by green belt land and also existing industrial and commercial enterprises. The Cragg Wood Conservation is located on the far side of the culverted bridge outside of the application site. The proposal is not envisaged to have any significant effects upon the setting of this conservation area. Overall the replacement of the former Clariant works with a modern low rise housing development should enhance the conservation area. The site is accessed via an existing point of entry from Calverley Lane. The site will also have a secondary vehicular access on to Low Hall Road. The bus service that will service the site will enter and exist the site via Calverley Lane. The bus will loop around the internal road system of the site.
- 10.3 The houses are arranged around the central internal spine road. Off the spine road would be a series of individual cul de sac. The masterplan has arranged houses fronting both Low Hall Road and Calverley Lane. The houses are of a traditional design and appearance. There is an Arts and Crafts theme to the styling of the properties with the use of render on the walls and timber within the gable roof areas. The houses fronting Calverley Lane are to be constructed out of natural stone and slate. These houses will provide a good setting for the site and will relate well to the stone properties on the opposite side of Low Hall Road and further up Calverley Lane facing the site. The remainder of the houses within the site will be constructed out of artificial stone and slate. The houses are mostly two storeys in height there are some two and half storey houses with small dormers. The scale of the houses is considered an improvement from the previous factory located on site. The proposed retail units will be located in the centre of the site. They will have two floors of flats above the ground floor retail units. These two buildings will be the only three storey buildings on site. The design and appearance of these blocks is considered in keeping with the general style and appearance of the estate. The extra height is considered acceptable and will not look out of place, particularly as they are located centrally within the site.
- 10.4 The scheme has been designed to provide usable areas of public open space within the development. Two areas of public open space are located in the centre of the site. The Village green area will provide the flat and open space that can be used by future residents and other local residents for informal play and recreation. This central area of open space is bound by a crescent of dwelling houses that front on to the open space area. The area of open space will be lined with semi mature trees to give it a sense of place. The second area of open space located adjacent to the Village green has several existing mature trees located within it and will provide a pedestrian link from the Calverley Lane side of the site through to the central area of the site and into the Village Green. To the South of the site will be located another large area of open space. This is fronted by houses to provide over looking and is

bound to the south by the Railway and River. The total size of the areas of open space provided meets with the policy requirements laid out with the Supplementary Planning Guidance for Greenspace provision. The areas are considered well planned and connected. The houses of the estate will provide natural surveillance.

- 10.5 The internal road layout has been designed to be wide enough so that the main spine road can be planted with street trees that will be part of the adopted highway. The central spine road should be over 13metres wide with houses facing on to it from either side of the road. The houses types vary in terms of roof styles and features such as single and two storey bays and the use of render of some properties should add visual interest. The variation in houses types combined with the changes in building lines of the rows of houses should help break up the continuous feel of the internal spine road. This variation of houses types should also add to the sense of place being created. The width of the road combined within the street trees should help to create a sense of place and identity for this development. The main road junctions within the site have also been designed to create a feature that are similar to the 'squares' found in the centre of small settlements. The use of block paving, raised tables and tree planting within these 'squares' should also help create the sense of place.

Residential amenity

- 10.6 The layout of the development is considered to provide future occupiers with an attractive traditional housing estate located within a semi rural location. The layout is designed to make the internal road network simple and legible. The creation of usable areas on site of public open space will make easy access to recreation for families. In addition the site is adjacent to the Green Belt and across Calverley Lane is located sports playing fields. The developer will be improving the existing pavilion building and maintaining this building for community and sports use. In addition the developer is also proposing to construct a small play area for young children. This play area will be within the curtilage of the playing fields but should not affect the usability of the playing fields. The play equipment proposed is designed for young children.
- 10.7 The proposed houses all have gardens that meet or more typically are in excess of the guidance within Neighbourhoods for Living SPG of two thirds gross floor area to private garden. The houses all have dedicated off street parking with a mixture of garages and driveways. The proposal layout has been designed to ensure that houses will not over look one and other and that the houses are arranged on level platforms so that they do not create over bearing or dominant relationships with each other.
- 10.8 The site will benefit from a bus service that will have a 30 minute service 7 days a week. This service will connect the development to Horsforth town centre. The sustainability and highway matters which were dealt with at the Outline stage. However, it is note worthy to mention the bus service as this developer will fund this for a 10 year period and it should provide future occupiers with an alternative means of transport to get into the town centre. This bus service as with the areas of public open space will be accessible to existing local residents as well as future occupiers. In addition the small retail units will also provide a good local and convenient amenity to both future occupiers and existing neighbours.
- 10.9 The proposed development is not envisaged to have any significant impacts on neighbouring residents with regard to issues of over looking and loss of privacy. The issues around off site car parking and highway safety were addressed during the Outline stage. The impact on the local community from the construction phase of the

development will be mitigated through planning conditions to control hours of construction and location of site contractors cabins and equipment etc.

Highways considerations

- 10.10 The proposed masterplan has been designed to ensure that all houses have sufficient off street parking. On the whole each dwellinghouse has 2 off street car parking spaces in the form of garages and driveways. A planning condition is attached to ensure that the garages are kept for the storage of cars and cannot be converted to habitable rooms without prior planning permission. In the communal situations, notably around the flats and retail units, parking is provided in lay bys off the main spine road.
- 10.11 The scheme has been designed to ensure that a bus can enter and loop around the estate road and exit via Calverley Lane. Refuse vehicles can also access the site including the cul de sacs. The road layout has also been designed so that if the Riverside Mills site comes forward the internal spine road can connect to this site. This section of road is also wide enough to facilitate a bus to enter the Riverside Mills site from the internal spine road. The developer has committed to funding any traffic regulation orders that may be required around the junction of Calverley Lane and Low Hall Road and also around the proposed new access on to Low Hall Road. The developer has also provided a commitment to dedicate an area of land near to the culverted bridged over Low Hall Road at the north western corner of the site to ensure that a continuous footpath for pedestrians can be provided should Riverside Mills come forward for redevelopment to connect to their site on Low Hall Road.
- 10.12 Overall the proposed layout complies with the car parking requirements and is acceptable for a highway safety point of view.

Other matters

- 10.13 As part of the Outline planning permission and in accordance with the Section 106 agreement the developer is providing 15% of the total number of dwellings as affordable housing (50 units to be affordable in total). The houses are located in a pepper potted arrangement in 4 main areas throughout the site. The affordable housing is a mixture of flats and 2 and 3 bedroom houses. There will be a 50/50 split between properties for sub market tenure and social rent. This complies with the planning policy requirements.
- 10.14 The developer is also required through the outline planning permission to upgrade the existing sports pavilion and playing fields located on the opposite side of Calverley Lane. The developer will redecorate and insulate this pavilion building. The pavilion building will be made available for local sports teams and also for wider community use. The developer has proposed a management committee to oversee the running of this facilitate and has also provided funds to maintain the pavilion and pitches. The local sports team who use these facilities will also be required to pay towards this. Sport England are currently objecting to the application around this matter but are in receipt of further information from the developer in relation to these matters and a verbal update will be brought to Panel on the outcome of these discussions.
- 10.15 The developer has also submitted details of the allotment scheme which the Outline permission requires them to do. These allotments will be located at the southern end of the playing fields and will be managed and maintained by the management company appointed by the developer.

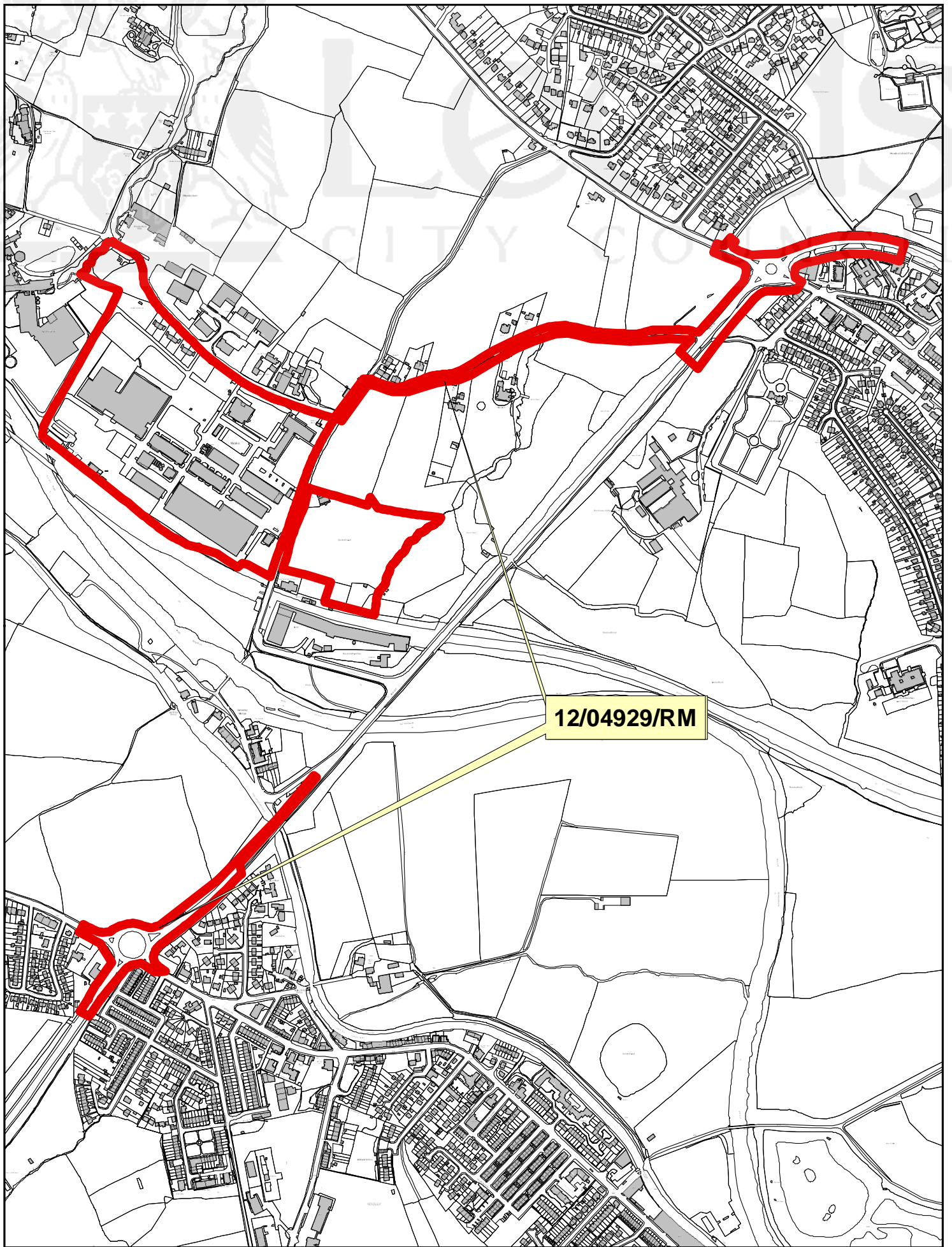
- 10.16 The developer will be constructed on the dwellings to meet Code for Sustainable Homes level 3 in accordance with their outline planning permission condition. They will also reduce CO2 emissions from the development by 20% below current Building Regulations. They are also proposing to install Air Source Heat Pumps to provide Low and Zero Carbon Technologies on site. These units will create 10% of the energy requirements on site.
- 10.17 The Council's Drainage Engineers and the Environment Agency have both assessed the details of the proposals. They have no objections to the proposed methods of drainage and also do not consider the proposal will add to flood risk in the area. The detail of the surface water run off scheme has been conditioned.

11.0 CONCLUSION:

- 11.1 After careful consideration of all relevant planning matters it is considered that the proposed details of Scale, Layout, External Appearance and Landscaping of the development is acceptable subject to the imposition of suitable conditions. The proposal is therefore recommended for approval.

Background Papers:

Application file;
Certificate of Ownership.



SOUTH AND WEST PLANS PANEL